

ITEM 2

DRAFT PLANNING PROPOSAL: FORMER CORRIMAL COKE WORKS,
27 RAILWAY STREET, CORRIMAL

In October 2017, Council received a draft Planning Proposal request for 27 Railway Street, Corrimal, known as the Former Corrimal Coke Works site. The draft Planning Proposal request seeks to rezone the subject site from IN3 Heavy Industrial, RE2 Private Recreation and SP2 Infrastructure (Road), to R3 Medium Density Residential, RE1 Public Recreation and SP2 Infrastructure (Road) with associated changes to the minimum lot size, height restrictions and floor space ratio maps, accordingly.

This report presents the preliminary assessment of the Planning Proposal request and recommends that Council resolve to submit a draft Planning Proposal to the NSW Department of Planning and Environment seeking a Gateway determination subject to the proponent providing additional information prior to a public exhibition period of a minimum 42 days.

RECOMMENDATION

- 1 A draft Planning Proposal be prepared for Lot 1 DP 795791, Lot 5 DP 749492, Lot 11 DP 749492 and Lot 126 DP 598190 known as the former Corrimal Coke Works including part of former Lot 12 DP 749492 Cross Street to amend Wollongong Local Environmental Plan 2009 as follows:
 - a Amend the Zoning Map as follows:
 - i Rezone Lot 1 DP 795791 from IN3 Heavy Industrial to R3 Medium Density.
 - ii Rezone Lot 5 DP 749492 to part R3 Medium Density Residential and part RE1 Public Recreation.
 - iii Rezone Lot 11 DP 749492 from SP2 Road Infrastructure to RE1 Public Recreation.
 - iv Rezone part of former Lot 12 DP 749492 from RE2 Private Recreation to R2 Low Density Residential.
 - vi Rezone the Cross Street Road Reserve from SP2 Infrastructure (Road) to R2 Low Density Residential.
 - vii Retain Lot 126 DP 598190 as SP2 Infrastructure (Road) (Attachment 4).
 - b Amend the Floor Space Ratio map to introduce a varying floor space ratio of between 0.75:1 and 2.5:1 (Attachment 4).
 - c Amend the Height of Buildings Map to introduce a varying maximum height of 13m and 24m, throughout the site (Attachment 4).
 - d Amend the Minimum Lot size Map to introduce a minimum lot size of 149m² for the R3 Medium Density zone.
 - e Amend the Heritage Map and Heritage Schedule to include identified significant fabric (coke ovens, chimney, stacks) as listed items of Local significance.
 - f Amend the Natural Resource Sensitivity – Biodiversity Map to identify the two significant native vegetation communities.
 - g Amend the Riparian Map to identifying the modified riparian corridor reflecting the proposed creek re-alignment and its integrating with open space resources including pedestrian and cycle paths potentially linking with the broader green link network.
 - h Amend the Key Site Map to identify the site as a Key Site under clause 7.18 Design Excellence.
 - i Amend Schedule 1 Additional Uses to incorporate additional uses for ‘food and drink premises’, and ‘shop’, limiting the size of the premises to a maximum of 150sqm, identifying a site specific location within a 100m radius of the train station.
 - j Incorporate a 5% Affordable Rental Housing target.

- 2 The draft Planning Proposal be forwarded to the NSW Department of Planning and Environment for a conditional Gateway determination, requesting that the following additional studies and information that will inform the proposed planning controls, be submitted prior to public exhibition:
 - a A Geomorphological report from a suitably qualified geomorphological expert to appraise the proposed watercourse realignment and verify that the proposed design (including alignment and channel dimensions) will enable a sustainable channel pattern and form and long term channel stability, and to provide advice on measures required to be incorporated into the design of the re-alignment in order to ensure these outcomes.
 - b The following details are required in relation to the flood modelling:
 - i Plans showing manning's roughness values used in the pre and post development flood modelling;
 - ii Details of the WBNM modelling including catchment plan, input data, catchment routing, structures, IFD data, and results;
 - iii Plan showing 2D flood model domain and boundary condition locations, including method, type and location of each model inflow;
 - iv Details of the hydrograph used at each inflow location; and
Plan showing comparison between flood levels predicted by Cardno model and those predicted by Council's adopted model, demonstrating parity between the two models as stated in Section 4.2.1 of the report by Cardno.
 - c The submitted flood study shall be amended to include modelling of 'Risk Management' blockage factors for the 1:100 ARI flood event, and mapping of the Flood Planning Area (FPA), i.e. the area of land below the Flood Planning Level (FPL), being the level of a 1:100 ARI (average recurrent interval) flood event plus 0.5 metres freeboard as defined in the Wollongong LEP2009 and is to include the correct blockage factors for Structures 48 (North Corrimal Creek, D/S Railway Street) and 1 (Carr Creek, Memorial Drive). Structure 48 is a Class 3 structure and requires a design blockage factor of 40%, and Structure 1 is a Class 1 structure and requires a design blockage factor of 70%. The flood modelling and Table 4-2 of the Flood Study report shall be updated accordingly.
- 3 Should a Gateway determination be issued, consultation be undertaken with the following agencies and stakeholders during public exhibition:
 - a Crown Lands and Water Division;
 - b EPA;
 - c NSW Office of Environment and Heritage;
 - d NSW Heritage Council;
 - e NSW Rural Fire Service;
 - f Department of Education and Communities;
 - g Endeavour Energy;
 - h Transport for NSW – Roads and Maritime Services;
 - i Sydney Water;
 - j Sydney Trains;
 - k RailCorp;
 - l Department of Fisheries;
 - m Illawarra Local Aboriginal Land Council;
 - n SES; and
 - o National Trust of Australia – Illawarra Shoalhaven Regional Branch.
- 4 The draft Planning Proposal be exhibited for a minimum period of 42 days.
- 5 The NSW Department of Planning and Environment be requested to issue authority to the General Manager to exercise plan making delegations, in accordance with Council's resolution of 26 November 2012.

- 6 The following additional information be submitted before or during the exhibition period, to enable Council to consider the information prior to determining whether to finalise the Planning Proposal:
 - a A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan addressing the following:
 - i Building Heights;
 - ii Floor Space Ratios;
 - iii Lot Size;
 - iv Building envelopes;
 - v Road widths;
 - vi Public spaces;
 - vii Streetscapes;
 - viii Housing types;
 - ix Connectivity and Access;
 - x Views and Vistas;
 - xi Urban Form Design development
 - x clause 7.18 Design Excellence of the Wollongong LEP 2009 (key Site requirements).
 - b Advice from the NSW Heritage Council for comment in relation to:
 - i The Archaeological significance of the site and the potential requirements and implications of archaeological impacts from future development under Section 140 of the NSW Heritage Act 1977.
 - ii The potential for the site to be listed on the State Heritage Register (in light of the findings of the Biosis Report).
 - c Further investigation of the relevant wastewater system capacity identifying that the local system is able to accommodate the additional development, including the consideration of onsite recycling and reuse as part of water cycle management/water services in the development site.
 - d Review of the local stormwater system ensuring there is adequate capacity to transfer runoff during heavy wet weather events which result in local flooding.
 - e Hydrological review in relation to the rail corridor bridge to the south east of the site.
 - f An amended Traffic Impact Assessment is to be prepared to better address the likely impacts of the development on the surrounding roads, including the rail level crossing and addressing the requirements of Table 2.1 of the RTA Guide to Traffic Generating Development.
 - g Review of Sydney Trains 33kV Aerial Line asset on the western side of the rail corridor and the proximity/impact of proposed development.
 - h A Rail and Road Acoustic and Vibration Assessment be prepared.
 - i An Aboriginal Cultural Heritage Assessment be prepared.
 - j A Conservation Management Plan for providing for the long term conservation of significant Coke Works heritage components be prepared.
 - k Agreement to provide at least 5% Affordable Rental Housing within the development, and advice on the proposed management arrangements of the dwellings and the proposed housing needs sectors to be targeted.
 - l A Revised Ecological Impact Assessment be prepared addressing the following:
 - i Potential impacts to micro bats roosting on the site; and
 - ii Potential for Green and Golden Bell Frog habitat on the site.

- m A revised Remediation Action Plan be prepared addressing:
 - i including further sampling under the structures on the site;
 - ii for PCB's associated with the powerhouse building and transformers;
 - iii address exceedance of ecological investigation levels; and
 - iv address materials containing SMF and Crystalline Silica associated with the coke ovens.
- n Advice on the proposed facilities and / or infrastructure, including costings, that are proposed to be incorporated into a draft planning agreement.

REPORT AUTHORISATIONS

Report of: Wayde Peterson, Manager Environmental Strategy and Planning (Acting)
Authorised by: Andrew Carfield, Director Planning and Environment - Future City and Neighbourhoods

ATTACHMENTS

- 1 Aerial Location Map
- 2 Existing Zoning Map
- 3 Concept Plan
- 4 Proposed Zoning and Proposed LEP Amendment Maps

BACKGROUND

Subject Site

The draft Planning Proposal request applies to the land known as 27 Railway Street, Corrimal, land identified as Lot 1 DP795791, Lot 5 DP 749492, Lot 126 DP 598190 and Lot 11 DP749492 formerly known as the Corrimal Coke Works (Attachment 1). The subject site is approximately 18.2 hectares in area.

The subject site is bounded by the main southern railway line on the east and a dual carriageway (Memorial Drive) on the west, Railway Street to the north and Towradgi Creek to the south. The site is generally flat to gently undulating south towards Towradgi Creek. Towradgi Creek generally forms the southern boundary of the site and drains in a west to east direction. A tributary of Towradgi Creek traverses the site and the western portion of the site is occupied by scattered bush and stockpiling areas.

The coke works operated for over 100 years and was the longest continuously operated coke works in the world. The coke works was a key component in the establishment of Corrimal as a suburb and ensured the economic health of the locality as a continuous employer of local families. The site comprised coking ovens, stacks, an artificial dam for water supply and related infrastructure, as well as coal and coke stockpiling areas.

The site is currently largely zoned IN3 Heavy Industrial and RE2 Private Recreation with a small portion of SP2 Infrastructure (Road) in the north east corridor (Attachment 2). The site is largely surrounded by low and medium density residential zoning to the north, north-east and east of the site, with a small provision of light industrial uses immediately to the north. Open space adjoins the site to the south and west.

The coke works ceased operation on 1 April, 2014, as it was no longer financially viable. Part of the Coke Works site (Lot 5 DP749492) is currently zoned RE2 Private Recreation. This land was previously owned by the Corrimal Leagues Club. It was originally owned by Illawarra Coke Company (ICC), sold to the Club in the early 1970's and purchased back by ICC in 2008 when the Club was under administration. However, it always remained as part of the coke works operation leased from the Corrimal Leagues Club.

Since the closure of the coke works in 2014, the site has become a vacant and dilapidated block of land. As part of the decommissioning process of the coke works a large proportion of the components which formed the coke works have been removed. The physical remains have fallen into poor condition through a combination of vandalism, theft and weathering by natural elements.

Regional and Local Context

Corrimal is located within 6.5km of Wollongong City Centre which provides access to a diverse range of services, employment opportunities and the University of Wollongong. The site is positioned close to education and recreation facilities such as the Corrimal Public School, Corrimal East Public School, Robert Ziems Park, Corrimal Memorial Park, Towradgi Beach Park, Bowls and Recreation Club, and sits within 1 kilometre of the Corrimal Town Centre.

A Councillor Briefing on the proposal occurred on 12 February 2018.

PROPOSAL

INDICATIVE DESIGN CONCEPT

The draft Planning Proposal request was accompanied by an indicative design concept report prepared by E8 Urban which illustrates a concept subdivision and residential development option which could be achieved within the parameters of the proposed Planning Proposal (Attachment 3 – one page concept design).

The indicative Concept Plan illustrates a range of lot sizes and the intent to deliver a diverse housing product outcome. It identifies the provision of potentially 190 lots, achieving 736 dwellings made up of 80 rear loaded terraces, 90 front loaded semi-detached dwellings, three front loaded detached dwellings, six manor homes in two manor house lots, and 556 apartments. The proposal has the opportunity to provide a diverse housing product including a mix of product (student accommodation, seniors housing, etc.).

The Urban Design Study is indicative only and does not present design controls. While it purports to form the basis of the draft Planning Proposal request, the detail relating to building forms and site layout in the concept plan have not been specifically translated in the proposed LEP Amendments. Further design development will be required to identify and formalise applicable built form controls across the site.

In addition, the concept design sets out a contextual analysis and identifies the following key features of the proposal:

- A new station plaza adjacent to Corrimal Rail Station
- A creek corridor with walking trails and play areas based on a creek realignment strategy
- A pedestrian and cycle route providing linkage opportunities to the town centre, community facilities and the beach
- An internal cycle path to connect Railway Street to Corrimal Station
- Vehicular access point off Railway Street
- Retention of identified threatened species communities
- Open space areas developed around pockets of remnant vegetation
- Indicative building envelopes
- A varying height limit across the site.

Other supporting documents submitted in conjunction with the draft Planning Proposal include:

- Planning Proposal Former Corrimal Coke Works (Elton 2017)
- Economic Impact Assessment (Hill PDA, 2017)
- Urban Design Study (E8Urban, 2017)
- Geotechnical Assessment (Douglas Partners, 2017)
- Preliminary Site Investigations (Environmental Strategies 2014)

- Phase 2 Detailed Site Investigations (Environmental Strategies 2014)
- Additional Environmental Assessment (Arcadis 2017)
- Remediation Action Plan (Arcadis 2017)
- Coke Ovens Environmental Assessment (Arcadis 2017)
- Historical Heritage Assessment (Biosis 2017)
- Aboriginal Archaeological Survey Report (Biosis 2015)
- Flora and Fauna Assessment (EcoLogical 2017)
- Preliminary Development Footprint advice regarding biodiversity (EcoLogical 2017)
- Corrimal Flood Study (Cardno 2017)
- Traffic and parking Impact Assessment (McLaren Traffic Engineering, 2017)
- Services Summary Report (ADW Johnson 2017)
- Feasibility Study of Gas, NBN and Electrical Services (Cardno 2015)
- Feasibility Study of Water and Sewer (Cardno 2015)
- Review of Illawarra Housing Market (SGS Economics and Planning, 2014 prepared for the (then) Department of Planning and Infrastructure).

The draft Planning Proposal request specifically seeks to amend Wollongong Local Environmental Plan 2009 in the following manner:

- Amend the Land Zoning Map to enable future residential and recreational development of the site as follows:
 - Rezone Lot 1 DP 795579 from IN3 Heavy Industrial to R3 Medium Density Rezone.
 - Rezone Lot 5 DP 749492 to part R3 Medium Density Residential and part RE1 Public Recreation.
 - Rezone Lot 11 DP 749492 from SP2 Infrastructure (Road) to RE1 Public Recreation.
 - Rezone part of former Lot 12 DP 74992 from RE2 Private Recreation to R2 Low Density Residential.
- Amend the Building Height Map, seeking a varying height limit from 13m to 24m.
- Amend the Minimum Lot Size Map, to permit a minimum lot size of 150sqm.
- Amend the Floor Space Ratio Map, providing no FSR across the site.
- Amend the land identified on the subject site as Natural Resource Sensitivity Map.
- Amend Schedule 1 to incorporate additional uses including 'food and drink premises' and 'shop'.
- Amend the Riparian Land Map and Foreshore Building Line Map, to modify the riparian corridor reflecting the proposed creek re-alignment and by deleting the foreshore building line associated with the original riparian corridor.

Key Issues for Consideration

The following key issues are relevant to the evaluation of the Planning Proposal request:

Land Use

Loss of Industrial Lands

The draft Planning Proposal request seeks to transition the heavy industrial site to medium density residential and open space. Consideration needs to be given to the loss of industrial lands in this location, and the potential impact on the provision of industrial lands in the Region.

The proponent submitted an assessment of the viability of the ongoing industrial use of the site, undertaken by Hill PDA (2017). The report concluded:

- 1 *“There is sufficient supply of well-located industrial land in the Wollongong LGA without the subject site.*
- 2 *Increased demand in transport, storage and warehousing sectors growing but the subject site is unsuitable*
- 3 *Conversion to new industrial uses has been identified to be not viable.”*

The Employment Lands Guidelines for the Illawarra (2008, Department of Planning), comprises an adopted set of guidelines to support the intentions of the Illawarra Regional Strategy (2007), and to promote good planning and orderly development of the region's employment lands. Section 2 of the guidelines produces a set of “Employment Lands Principles” to help guide land use planning decisions by NSW Government and Councils. The general principles in Section 2 of the document include promoting the Regional Strategy, promoting commercial centres hierarchies, and to cluster businesses and industries with similar environmental impacts and business synergies to improve business efficiency and identity and reduce land use conflicts. The principles of the guidelines also seek to preserve lands of regional significance for employment generating uses.

The heavy industrial use ceased in 2014 and the site is not currently providing active employment. The site is isolated as an industrial site, sitting within a residential context. As such, its ability to sustain heavy industrial land uses is limited. It is generally isolated from light or heavy industrial uses. Although to the north of the site (Ruddock Street) comprises light industry land uses including for example: kitchen renovator, smash repairs, commercial fridges, welding and fabrication, pet and produce supplies.

It is located adjacent to Memorial Drive and the rail corridor (and historically was connected to the rail network) which would facilitate the transport of goods and the movement of employees.

The site does not have a reasonable vehicular capacity to accommodate a broad range of heavy industrial or light industrial uses given its limited (singular) access via Railway Street.

The site is not noted to be strategically important industrial land as the majority of industrial land in the Wollongong LGA is found in Unanderra, Kembla Grange and Port Kembla offering lower cost and comprising better access than the subject site.

Council must consider whether or not the site is able to transition to a residential zoning. The site sits landlocked by a mix of residential zones, open space/community facilities and within close proximity to a retail/commercial hub. Its access is limited to one entry/exit point being from a local street (Railway Street). The industrial zone in effect has been pushed out over time, as its capability of ensuring minimal land use conflict upon the adjoining residential urban environment has increased. As such, a residential land use is likely to reduce the incidence of incompatible land uses in the area.

The site presents an opportunity for targeted residential redevelopment and improvement of public open space areas. On balance it is considered that the site presents strategic merit for residential redevelopment and the potential for good urban design outcomes, interpretation of identified heritage items, whilst being capable of meeting Council's objectives of the R3 Medium Density zone in particular:

- *“To provide for the housing needs of the community within a medium density residential environment.*
- *To provide a variety of housing types within a medium density residential environment.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.”*

It is recommended that a site specific DCP Chapter be developed for the subject site in collaboration with the proposed Concept Design and proposed Wollongong LEP amendments. To achieve high quality design development in the public interest and contribute to the economic life and vitality of

Corrimal, it is further recommended that the subject site be nominated as a 'key site' under the provisions of Clause 7.18 Wollongong LEP 2009 - Design Excellence 'Key Site', the objective being to deliver the highest standard of architectural and urban design for the re-development of the site.

Additional Permitted Uses

The draft Planning Proposal has requested additional permitted uses to be included within the R3 Medium Density Residential zone as an amendment to Schedule 1. This includes Food and Drink Premises; Home Business; and Home Occupation; and Shop. Definitions are provided below:

"Food and Drink Premises" means premises that are used for the preparation and retail sale of food or drink (or both) for immediate consumption on or off the premises, and includes any of the following:

- (a) a restaurant or cafe,
- (b) take away food and drink premises,
- (c) a pub,
- (d) a small bar.

Note. Food and drink premises are a type of retail premises—see the definition of that term in this Dictionary."

"home business" means a business that is carried on in a dwelling, or in a building ancillary to a dwelling, by one or more permanent residents of the dwelling and that does not involve:

- (a) the employment of more than 2 persons other than those residents, or
- (b) interference with the amenity of the neighbourhood by reason of the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, traffic generation or otherwise, or
- (c) the exposure to view, from any adjacent premises or from any public place, of any unsightly matter, or
- (d) the exhibition of any signage (other than a business identification sign), or
- (e) the sale of items (whether goods or materials), or the exposure or offer for sale of items, by retail, except for goods produced at the dwelling or building,

but does not include bed and breakfast accommodation, home occupation (sex services) or sex services premises.

Note. See clause 5.4 for controls relating to the floor area used for a home business.

"home occupation" means an occupation that is carried on in a dwelling, or in a building ancillary to a dwelling, by one or more permanent residents of the dwelling and that does not involve:

- (a) the employment of persons other than those residents, or
- (b) interference with the amenity of the neighbourhood by reason of the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, traffic generation or otherwise, or
- (c) the display of goods, whether in a window or otherwise, or
- (d) the exhibition of any signage (other than a business identification sign), or
- (e) the sale of items (whether goods or materials), or the exposure or offer for sale of items, by retail,

but does not include bed and breakfast accommodation, home occupation (sex services) or sex services premises.

"Shop" - means premises that sell merchandise such as groceries, personal care products, clothing, music, homewares, stationery, electrical goods or the like or that hire any such merchandise, and includes a neighbourhood shop, but does not include food and drink premises or restricted premises.

Note: Shops are a type of retail premises—see the definition of that term in this Dictionary.

The inclusion of *Home Business* and *Home Occupation* as additional permitted uses is permitted through SEPP Exempt and Complying Development Code. As such amendment to Schedule 1 for these uses is not required.

Introduction of additional permitted land uses of 'Shop' and 'Food and Drink Premises' is supported in principle, with consideration given to location, extent and scale of these uses. It is critical that land uses allowed on the site do not present a conflict with the viability of the Corrimal Town Centre.

A degree of flexibility in addition to the 'Food and Drink Premises' proposed land use, should be provided to ensure that premises are not vacant and the plaza area and Corrimal Railway Station are utilised. It is considered appropriate that the additional use of 'Shop' also be included with a placed limitation of size within the amendment, allowing variation and the communities needs to be met. Concern has been raised to ensure that the provision of these types of land uses be restricted to within 100 metres, reasonable proximity of 'Heritage Plaza' and Corrimal train station.

Should the land use definition for *Shop* be supported, it is recommended that each premises be limited in size to a maximum of 150sqm in floor area and be located within reasonable proximity (100m) of Corrimal train station. This will ensure minimal impact upon the existing Corrimal Town Centre and provide a balanced approach of support for the proposed residential neighbourhood.

Urban Design Considerations

Floor space ratio

The site currently has no Floor Space Ratio (FSR) provisions. Floor space ratio is the ratio of the gross floor area (of a building or development) to the site area. FSR helps to guide built form outcomes and establish an expectation for development across the site. FSR controls are used to regulate the scale and character of built form. As part of the development assessment process it is used to determine whether a development proposal complies.

The applicable FSR is shown on the Floor Space Ratio Map and works independently of the zoning of the land, although generally reflects the zone. The Floor Space Ratio can vary across a zone, within a town centre or on a large development site. The FSR for the typical R3 Medium Density Residential zone, including the other side of Railway Street, is 0.75:1.

On a large site, such as the Coke Works site, the Floor Space Ratio can be measured at different scales, based on the overall site area, the area zoned R3 Medium Density Residential, proposed superlots or individual development lots.

The draft Planning Proposal request has requested that no FSR apply to the site. Council officers consider that it is appropriate for an FSR to guide future development outcomes.

The Urban Design Study proposes a mix of development scales/density across the site. The report proposes a total gross floor area of 78,269m², which is divided into 14 superlots and 736 dwellings, and is summarised in the following table.

Super lot ID	Lot area		Rear loaded terrace	Front loaded semi	Front loaded detached	Manor house	Apartment units	Total dwellings	Floor space (m2)	FSR	Proposed height
		Dwelling Type	F	E	A	H					
		Lot area	180	225	350	380					
		Floor space	108	140	221	257	100				
		Dwellings / lot	1	1	1	3					
		Lot FSR	0.60	0.62	0.63	0.68					
1	4434						62	62	6170	1.39	3-4
2	4555						73	73	7252	1.59	3-4
3	5617						83	83	8334	1.48	3-4
4	6808						154	154	15355	2.26	4-6
5	7152		38				0	38	4104	0.57	2
6	3960						44	44	4360	1.10	3-4
7	8230			32			0	32	4480	0.54	2
8	6298		10	16		1	0	29	3577	0.57	2
9	5361		22				0	22	2376	0.44	2
10	6564			22			21	43	5156	0.79	2-3-4
11	5104		10	11		1	0	24	2877	0.56	2
12	6026						104	104	10430	1.73	3-4
13	3998			9	3		0	12	1923	0.48	2
14	1536						17	17	1680	1.09	2
Building 01									195		
Totals		75643	80	90	3	2	556.57	735.57	78269		
										Super Lot FSR	1.03
										Ave super lot FSR	1.04
										R3 FSR	0.69
										Site FSR	0.43

When considering the proposed development across the entire 18.22 hectare site, equates to a Floor Space Ratio of 0.43:1, when considering the proposed R3 Medium Density Residential zoned land (11.42 hectare) the FSR increases to 1.03:1. As indicated in the table the FSR for the 14 superlots varies between 0.44:1 and 2.26:1 with an average of 1.04:1. The proposed four dwelling types have an FSR of between 0.6:1 and 0.68:1.

It is considered that the FSR can be increased beyond 0.75:1, to enable the implementation of the vision, the provision of a variety of housing products and to provide a clear intentions of the future development.

A draft Floor Space Ratio Map has been prepared reflecting the concept plan, showing FSR of 0.75:1, 1.5:1, 2:1 and 2.5:1 for different part of the site. The FSR maps forms part of the draft Planning Proposal (part of Attachment 4).

Building Height

The indicative Concept Plan identifies the varying height limit throughout the site of 13m – 24m, as it identifies a two storey (9m) built form fronting Railway Street. Taller buildings are proposed within the centre of the site adjacent to Corrimal Train Station and superlot 4, (a maximum height control of up to 24m) a built form of four to six storeys. Whilst the northern and southern portions of the site provide a varying built form (terrace development) of three to four storey, (approximately 13m).

The draft Planning Proposal request identifies the intention to nominate a varying building height control of 13m (throughout the site) and 24m (within the central portion of the site) over the subject site.

Consideration is given to the compatibility of the proposed varying height limit within the surrounding area. Adjoining the site is a mix of permitted building height zones with a minimum of 9m principally low density residential, 11m within Ruddock Street, 13m in the existing R3 Medium Density Residential zone north of the subject site and adjacent to the eastern side of Corrimal Train Station and 15m – 20m within the Corrimal Town Centre commercial/retail sites. A new development known as 'The Village' fronting Railway Street (on the former Corrimal Leagues Club site) has been approved with an approved maximum height limit of 13m being generally of a four storey nature.

Appropriateness of building height needs also to consider heritage matters. The National Trust and Council's Heritage Planner have raised concerns with regard to the visual impact upon the potential heritage structures on site, in particular, Quench Towers (36m) and Brick Chimney (30m) and the surrounding environment. They consider a more realistic maximum height limit of 16m (or five storeys) to be more reasonable for the site and its location.

However, taking into consideration the bulk, scale and height of the existing industrial structures on site (Quench Towers 36m, Brick Chimney 30m) adjoining redevelopment 'The Village' (13m height limit four storey development) Corrimal Town Centre (20m) and the current surrounding residential landscape, the proposed varying height limit with a lower built form at the sites boundaries and a proposed isolated 24m height limit within the centre of the site (adjacent to Corrimal Railway Station) is considered to be a reasonable design solution.

The proposed 24m height limit or six storeys within the centre of the site is considered to be reasonable and functional. It will stand alone presenting opportunity for flexibility with ground level local shops and food and drink premises, affordable housing, whilst being surrounded by a mix of two, three and four storey diverse housing developments integrating into the current low density surrounding residential environment. The varying height limit within the site is supported as it will permit better integration with the surrounding locality whilst also encouraging Council's future growth for Corrimal as the northern suburbs hub.

To provide greater certainty on the future built form, the maximum building height limit could be reduced to 9m in the superlots (5, 7, 8, 9, 11, 13 and 14) where two storey development is proposed. However, at this stage the scale and density of the development is proposed to be controlled through the Floor Space Ratio Map, and the proposed Height of Building Map reflects the proposed 13m and 24m height limits.

Further design detail is required to ensure appropriate height levels will apply throughout the site. The detail is to be incorporated within the design guidelines and DCP Chapter, in support of the proposed Wollongong LEP 2009 Height of Buildings map amendment.

Lot Sizes

Corrimal is earmarked as the major urban hub of the northern suburbs, as identified within the Illawarra Shoalhaven Regional Plan 2015, Community Strategic Plan and Corrimal Town Centre Plan 2015. Corrimal is capable of infill development not only as a result of its location but also the services and infrastructure that are capable of supporting the subject site and the locality.

The proponent seeks to amend Wollongong LEP 2009, Lot Size Map, to permit a minimum lot size of 150sqm across the subject site. The current industrial zoning provides a minimum lot size of 2 hectares, whilst the surrounding residential locality provides a minimum lot size of 449sqm within the identified R2 Low Density Residential and R3 Medium Density Residential zones respectively.

The reduction in lot size has the potential to encourage a diverse mix of housing types including residential flat buildings, attached and semi-detached dwellings, a form of housing stock currently lacking in the area. A reduced minimum lot size is considered to support Council's nominated residential objectives in particular:

"The key objectives of Chapter B1 – Residential Development of the DCP are:

- (a) To ensure a high standard of residential development within the City of Wollongong LGA.*
- (b) To encourage new residential development that is sympathetic to the existing streetscape and neighbourhood character of a particular locality.*
- (c) To encourage residential development that reflects the desired future character of individual suburbs within the Wollongong City LGA.*
- (e) To encourage innovative housing design and energy efficient housing which embraces the highest possible architectural, environmental and amenity standards.*
- (f) To promote residential development that achieves the principles of ecologically sustainable development.*
- (g) To encourage a mix of housing forms within the city to assist in achieving urban consolidation initiatives particularly in localities close to business centres and railway stations and to assist in providing housing affordability."*

The minimum lot size of 150sqm potentially has merit and is supported as an added incentive to encourage the development of a diverse housing product (terrace/townhouse development, attached/detached), encouraging the growth of a diverse community.

It is recommended that further design detail review the proposed housing typology and be identified within the nominated DCP Chapter guidelines.

Affordable Housing

The proposed redevelopment of the Coke Works site for housing, adjacent to the Corrimal Rail Station provides the opportunity to provide a range of housing densities, products and outcomes. The concept plan shows a mix of free standing dwellings, terrace style housing, apartment buildings of between two – six storeys in height. The proposed R3 Medium Density Residential zone also provides the opportunity for seniors housing.

The rezoning of the site, also provides the opportunity to consider whether affordable housing for students, key workers or social housing tenants should be included to contribute to the housing diversity of the precinct. These types of dwellings would need to be managed by a Community Housing Provider to provide housing at below the market rate. The large site provides the opportunity for rental dwellings to be scattered throughout the site, rather than being concentrated in one building.

One of the new objects of the Environmental Planning and Assessment Act 1979 is “*to promote the delivery and maintenance of affordable housing*”. At the moment this object is implemented via State Environmental Planning Policy No. 70 Affordable Housing (Revised Schemes). A Council needs to be listed in SEPP 70, before being able to include Affordable Housing provisions in its Local Environmental Plan. At the moment the SEPP lists three LGAs and the NSW Department of Planning and Environment has exhibited an amendment proposing to include a further five LGAs.

On 17 July 2017, Council considered a report on the draft Housing Discussion Paper and Council resolved (in part) to consider the use of SEPP 70 to achieve affordable housing provisions. This work is still occurring.

Nevertheless for this site, it is important that Council flag the intention that Affordable Housing should be provided as part of the development. Based on the concept plan, if an affordable housing target of 5% was introduced, this would equate to 36 dwellings, and could likely be achieved within the proposed FSR and Height Maps. If a target of 10% was introduced this would equate to 73 dwellings and may require a increase in FSR within a superlot. As noted, the submitted proposed building height limits could accommodate taller buildings without amendment.

Flood Planning

The ICC site is located within the lower reaches of the Towradgi Creek catchment. Towradgi Creek and North Corrimal Creek traverse the site and merge within the south eastern portion of the site. North Corrimal Creek enters the site from the northwest and traverses the site in a southerly alignment towards Towradgi Creek. Towradgi Creek flows in an easterly direction along the sites southern boundary. A significant area of the ICC site surrounding the creeks is considered to be flood prone, as such, the site and surrounding locality is subject to flooding. The existing watercourse within the portions of the site identified to be developed has been highly modified as a result of previous uses and infested with weed vegetation. A component of the proposal seeks the realignment of the existing riparian corridor/creek.

A report prepared by Cardno (Corrimal Flood Study, 28 June 2017) assessed that the flood risk levels currently present on site can be significantly mitigated to enable a Probable Maximum Flood (PMF) event free area of approximately 12.85 hectares. In addition the proposed creek realignment and mitigation works can be designed to convey major flows up to PMF event within North Corrimal Creek through the site.

Water Management Act 2000

The creek realignment is identified as a ‘Controlled Activity’ subject to the provisions of the Water Management Act 2000. Eco Logical Australia prepared a development footprint review identifying that the current creek has been heavily modified and degraded with 490m of waterway historically/previously realigned within the site.

The current creek alignment has degraded banks and is heavily infested with weeds. The proposal includes realigning the riparian corridor to alleviate flooding and to provide a suitable site area to support the intended residential development outcomes. The realignment proposes to divert 550m of channel from the residential neighbourhood alongside the western boundary (Memorial Drive) and return it to the existing creek downstream of the dam.

Realignment of Riparian Corridor

In support of the realignment of North Corrimal Creek to the western site boundary (currently traversing the centre of the site), the supporting Flood Study prepared by Cardno outlines the advantages and risks of adopting the watercourse realignment as follows:

“Potential advantages of adopting this watercourse realignment option are as follows:

- > Significantly improves flooding conditions within the existing Cross Street and Railway Street residential areas located upstream of the ICC site;*
- > Provides an opportunity for improved drainage of the existing Cross Street residential development;*
- > Easy connection of existing drainage on Memorial Drive to the proposed North Corrimal Creek without large culverts or pipe network;*
- > Opportunity to rehabilitate areas of the ICC site. Realigning the channel would offer the opportunity to rehabilitate a newly proposed VRZ (at the western boundary) with native vegetation. This could significantly improve conditions of the degraded riparian corridor that currently traverses the site;*
- > A large continuous parcel of flood free land can be achieved;*
- > Increase developable area in the north-east of the ICC site. The north-eastern site area is generally flood-free and therefore offers an ideal opportunity for development. Realigning the channel to the western ICC site boundary may further increase this flood-free area in the central site area, thereby increasing development opportunities and reducing existing flood hazards.*

Potential risks are as follows:

- > Substantial earthworks and topography modifications will be required. The FRMS&P 2003 recommends that large channelization schemes of this nature are not recommended due to a lack of significant benefits and the significant environmental and economic costs associated. In this case, however, realigning the channel could potentially improve environmental conditions by way of rehabilitating the riparian corridor and removing artificial fill introduced on the ICC site. Moreover, there are significant community benefits to local residents, particularly in Cross Street and Railway Street;*
- > Significant costs are likely to be incurred considering the significant size of the channel and the extent of rehabilitation required.”*

Council's officer consider the realignment of the creek acceptable, based on the establishment of a revegetated riparian corridor and approval from Department of Primary Industries - Water.

The Department of Primary Industries – Water, were notified of the proposal and subsequently reviewed the Riparian Report. General agreement with the assessment of the watercourses within the site and the recommended riparian outcomes was conveyed.

“Crown Lands and Water Division (CL&W) will undertake further assessment of the proposal at the development assessment stage. Future detailed design of the development is to be undertaken with consideration of the DPI Water 2012 Guidelines for riparian corridors on waterfront land.”

Surrender of Illawarra Coke Company Licence Environment Protection Licence

The EPA has advised that they have been regulating the Corrimal Coke Works site under an Environment Protection Licence. This licence relates to coke production activities. Coke production ceased in April 2014 and since that time the land has been dormant.

With the end of production and the proposed change in land use, Illawarra Coke Company applied to surrender this licence. In January 2018, NSW EPA, formerly provided notice of the surrender of the

license to Council. Wollongong Council is now the environmental regulator of the site. It should be noted, that the EPA will continue to assist Council to appropriately manage any environmental matters as the land transitions to a new use.

There are planning and development control processes in place for the management of any land contamination resulting from historical industrial activities at the premises. A thorough assessment process has been followed to date and the information submitted determined that no immediate land remediation is necessary for the existing site and uses. The contamination assessment process involved investigations of site contamination by consultants engaged by ICC. The investigations covered soil, groundwater, surface sediment and surface water contamination. It identified that there was contamination, consistent with coke making activities, but this contamination was not extensive and is contained within the boundaries of the site.

Site remediation works will be required to accommodate the new, more sensitive, residential/open space land uses proposed. The planning process incorporates formal systems to manage any further assessment and remediation which is likely to take place during proposed redevelopment.

Contamination

State Environmental Planning Policy 55 – Remediation of Land (SEPP 55), requires Council to consider whether land is contaminated and is required to be remediated before permitting sensitive land uses such as residential and recreation. The proponent subsequently submitted a Contamination Assessment Report, a Phase 1 Site Assessment Report, Phase 2 Detailed Site Investigation, Site Audit and a Remediation Action Plan.

The submitted Environmental Assessment Report prepared by Arcadis (12 April 2017) noted the following schedule of works, including:

- excavation of 35 test pits through the Berms across the site;
- sampling of surface water within the two main dams at the site (eight samples in total);
- sediment samples from each dam (six samples in total);
- soil samples (50 samples);
- groundwater monitoring; and
- ground gas monitoring.

The submitted reports identified six locations within the site that exceeded human health land use guidelines. The Remediation Action Plan recommends the preferred remediation strategy is excavation and offsite removal of the impacted material to render the site suitable for residential and open space uses.

Council's officers have reviewed the submitted documentation and noted:

“that the site has been subject to extensive disturbance over a long period of time in association with coke producing activities. The geotechnical aspects of these materials can be managed through routine earthworks with supplementary geotechnical advice to support the engineering designs. This can be worked in with the management of other site constraints which will be much greater issues to be addressed during design particularly environmental and flooding.”

It is recommended that the Remediation Action Plan be revised to incorporate the following detail prior to exhibition:

- i including further sampling under the structures on the site,
- ii for PCB's associated with the powerhouse building and transformers,
- iii address exceedance of ecological investigation levels,
- iv address materials containing SMF and Crystalline Silica associated with the coke ovens.

Heritage

The Draft Planning Proposal was supported by an 'Aboriginal Archaeological Survey Report' and 'Corrimal Coke Works Historical Heritage Assessment' (Biosis 2017). The reports noted:

"Heritage investigations have not found evidence of Aboriginal heritage on the site. However, significant post European heritage has been identified....."

The Corrimal Cokeworks have been assessed as possessing historical, aesthetic and social, technical rarity and representativeness significance at a local and state level. The elements which comprise the cokeworks contribute to varying degrees to this significance, the most important components being the C1 coke battery and the power house chimney which have been assessed as having a high level of significance. The C2 coke battery and potential archaeological remains of the c.1889 tramway have been assessed as having moderate significance. All remaining items although they assist with the interpretation of the processes undertaken at the cokeworks have been assessed as having little or no significance."

Council Heritage Officers have reviewed the submitted documentation and advised as follows:

'The draft Planning Proposal is supported by an "Aboriginal Archaeological Survey Report," prepared by Biosis and dated 28 October 2015. This report provides an assessment of the Aboriginal Archaeological Potential of the site and concludes that the site has low archaeological potential due to the level of past industrial disturbance. It is noted that this report provides no evidence of any Aboriginal Community Consultation and does not include any form of Aboriginal Cultural Significance assessment. This is a concern as the cultural significance of a site is a relevant consideration for planning proposals regardless of the archaeological significance/ and/or past disturbance of the site. This is particularly the case for a site which is located close by to (adjoining) a major coastal waterway in the area (Towradgi Creek) which is known to have significant Aboriginal Cultural Heritage Values.'

'The planning proposal is supported by a report, "Corrimal Cokeworks Corrimal NSW Historical Heritage Assessment", prepared by Biosis, and dated 1 August 2017. The report provides a preliminary assessment of the significance of the currently unlisted Corrimal Cokeworks Site, and provides a recommendation in support of the planning proposal. The submitted report appears to indicate that the Cokeworks Site is of State Heritage Significance.

....in finalising the Planning Proposal to give further consideration to the significance of the fabric. The intent being to allow a heritage listing for the significant structures and site components proposed to be conserved, to be formalised through the LEP heritage list and heritage map within the Planning Proposal. This will require the definition of a proposed curtilage boundary, and a clearer understanding of the significance and potential retention value of the significant components. It is general heritage practice to aim to conserve all fabric of "high" and "moderate" significance, though certainly it would be appropriate in this instance to consider the retention of at least representative fabric, that would allow for the interpretation and demonstration of the historic operation of the site, as well as its evolution during the extended life of the operation. Whilst heritage interpretation offers a potential means for achieving this objective, the retention of significant fabric should form an integral component of the interpretive process."

Having regard to the European and Aboriginal heritage identified at the subject site the following recommendations are made:

- 1 An Aboriginal Cultural Heritage Assessment is required to be provided prior to exhibition.
- 2 The draft Planning Proposal is to be referred to the NSW Heritage Council for comments in relation to:
 - a The Archaeological significance of the site and the potential requirements and implications of archaeological impacts from future development under Section 140 of the NSW Heritage Act 1977.

- b The potential for the site to be listed on the State Heritage Register (in light of the finding of State Significance in the Biosis Report), and the views of the NSW Heritage Council in relation to the Planning Proposal.
- 3 Subsequent to further investigation amend the Heritage Map to include identified significant fabric (coke ovens, chimney, and stacks) as listed items of Local significance and potentially State significance depending on the views of the NSW Heritage Council.
- 4 A Conservation Management Plan, which would provide for the long term conservation of the significant heritage components, proposed interpretive devices and any remnant archaeological components conserved through the process, be prepared and submitted prior to finalising the Planning Proposal.

Biodiversity

In support of the Planning Proposal a Flora and Fauna report was prepared by EcoLogical Australia Pty Ltd (September 2017). The study area has a long history of disturbance associated with its use as a Coke Works. As such, it was noted that the vegetation communities across the site have been subjected to moderate to high levels of disturbance including vegetation clearing/thinning of canopy layer, infestation of exotic species and modification of landform and soil profiles.

Nevertheless, an example of an endangered ecological community listed under the BC Act was identified. It was identified that the proposed residential rezoning will affect approximately 0.06 ha of Moist Blue Gum Blackbutt Forest, within the study area and 0.08 ha of Forest Red Gum Thin-leaved Stringybark Grassy Woodland which forms part Illawarra Lowlands Grassy Woodland an endangered ecological community under the BC Act.

Furthermore, one threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) was identified in the study area during survey. The Grey-headed Flying-fox is listed as vulnerable under the BC Act and EPBC Act. The Grey-headed Flying-fox were found occupying a small area of Moist Blue Gum Blackbutt Forest at the southern extent of the study area.

Planted individuals of one species listed under the BC Act and EPBC, 'Eucalyptus scoparia', was also identified within the study area, although these planted individuals outside their natural range do not represent constraints to the proposed rezoning. The study area was also considered to represent potential habitat for highly mobile threatened fauna including bats and migratory birds, although the proposed rezoning was considered unlikely to affect these species.

Council Officers have reviewed the submitted detail and considered it to be generally satisfactory. In addition it has been requested that an Ecological Impact Assessment be required to be prepared to assess the issues identified.

Traffic Implications

A Traffic, Transport and Parking Assessment prepared by McLaren Traffic Engineering & Road Safety Consultants was submitted with the draft Planning Proposal request. The review of the submitted Traffic and Transport Study identifies that it does not provide a comprehensive analysis of the likely impacts of the proposal upon the adjoining locality. Both Council and State Agencies have requested revised and additional documentation.

The concept plan identifies one primary vehicular access point from Railway Street. It is recommended that further investigation is required in terms of treatment of intersections, proposed improvements to Memorial Drive, traffic generation including the distribution and impact upon the existing railway level crossing.

Discussions have been held with the proponent advising that further detailed analysis is required in support of the proposed redevelopment including the need to communicate with RMS and Transport NSW to incorporate any future plans with regard to the level crossing and Memorial Drive.

Lot 126 DP 598190 located on Railway Street adjacent to the level crossing is zoned SP2 Infrastructure (Road) and has been earmarked for Council acquisition since 1974 to enable a bridge to replace the level crossing. The original submitted draft Planning Proposal and concept plan, proposed the rezoning

of this area and residential development. Following discussions with Council officers, the Planning Proposal was amended to retain the SP2 Infrastructure (Road) zone and the concept plan was amended to remove the residential development. The lot is also identified in the Wollongong LEP 2009 Land Reservation Acquisition Map for Council acquisition. It is envisaged that the acquisition will occur as part of the Development Application process, and the cost will be off-set from the development contribution. There is no timeframe or budget for the construction of the bridge and there has been no discussion with State Transport agencies on funding. It is likely that the 1974 design will need to be updated. Securing the corridor is an important step in providing for the future bridge.

The original concept had two access points to Railway Street with the proposed transfer of Lot 126 DP 598190 to Council for the future rail bridge, the number of intersections shown in the concept has been reduced to one

Other Minor Zoning Changes

The Planning Proposal seeks to incorporate the rezoning of a number of adjacent land holdings.

Lot 11 DP749492

The proponent has incorporated Lot 11 DP749492 within the Planning Proposal seeking to include this parcel of land approximately 66sqm in area within the proposed RE1 Public Recreation zone. The lot is currently zoned SP2 Infrastructure (Road).

No concerns have been raised with regard to the rezoning of this allotment/portion of the site and incorporating it within the Planning Proposal.

Part of former Lot 12 DP749492

In considering the rezoning of the subject site a minor anomaly was identified upon Council's Zoning Plan whereby part of former Lot 12 DP749492 is mapped as RE2 Private Recreation. As such, in order to rectify this anomaly it is sought to incorporate this amendment within this Planning Proposal amending the zoning to SP2 Infrastructure (Road) in accordance with Cross Streets zoning and existing use of the lot as a road.

Connectivity/Linkage

The proponent is seeking to encourage two primary linkage points as part of the sites re-development:

- 1 The primary pedestrian linkage is along 'Railway Street' connecting the site in an east west direction with the Corrimal Railway Station and Corrimal Town Centre.
- 2 At the southern end of the site a pedestrian/bicycle path linking the riparian corridor with the coast and Council's recreational/community facilities and Robert Ziem's Park.

This secondary linkage path is proposed to incorporate two underpasses within the existing bridge crossings under Memorial Drive and the second at the existing bridge crossing under the railway line. In this regard, further detail and investigation will be required with regard to implementation and construction of a bicycle/walking track/maintenance/surveillance including the concurrence of RMS and RailCorp for working in and around their infrastructure and NSW Fisheries with respect to impact upon Towradgi Creek.

A further point of connectivity through the centre of the site is to be considered. This will provide a direct link between the site, Corrimal Town Centre via Short Street the existing community facilities including Corrimal pool and Corrimal Train Station.

This is to be a consideration in the design and development phase of the proposal.

Development Contributions / Planning Agreement

The proponent has indicated the intent to enter into a Planning Agreement with Council. Further information will be required to ratify the details and structure of the agreement. It is unclear what works or facilities a Planning Agreement would include. Any on-site works may be covered by a Works-in-kind agreement. The proposed transfer of Lot 126 DP 598190 on Railway Street for the rail bridge could be

included in either option. Off-site works, such as the proposed footpaths would need to be designed and costed before being considered.

Based on a 1% Section 7.12 (formerly S94A) levy, the proposed \$230 million constructions cost (HillPDA), could contribute \$2,300,000 in development contributions.

CONSULTATION AND COMMUNICATION

Preliminary agency and internal staff consultation was carried out as part of the assessment of the draft Planning Proposal request.

Internal Stakeholders

Preliminary consultation was sought from Council's Specialist staff. There comments are noted as follows:

	COMMENT
Developer Contributions	<i>The site falls within the provisions of the Section 94A Contributions Plan and a contribution will be levied at DA staged based on the proposed cost of development.</i> <u>Comment</u> Proponent has flagged potential VPA agreement with Council.
Stormwater and Flooding	Revised documentation and further studies have been requested to be submitted should the Planning Proposal be supported.
Traffic	No objections in principle to the proposal subject to further revised detail being submitted.
Property	<i>We have no objection to this planning proposal, subject to concurrence from the other Divisions.</i>
Heritage	Comments as noted within the report.
Social Planning	Social Planning generally supports the Concept Plan to offer a diversity of housing to deliver both affordability and under-represented housing typologies in the Wollongong region.
Landscape Design	No objections raised.
Environment	The proposal is considered to be generally satisfactory with regard to the nominated Environmental issues subject to revision of nominated reports.
Geotechnical Services	No objections raised.
Subdivision	No objections raised at this preliminary phase of the Proposal.
Civil Asset Management	<i>An opportunity exists for an improved flowpath and drainage configuration that would reduce the likelihood of flooding for Cross Street properties and facilitate Council's provision of kerb and gutter.</i>

Consultation with Public Agencies

Preliminary Consultation has been undertaken. The following agencies/stakeholders were contacted and their comments are noted as follows:

	COMMENT
Rural Fire Services	No objections to the proposal.
Crown Lands and Water Division	No objections to the proposal.

	COMMENT
Office of Environment and Heritage	<p>In summary, we support the revitalisation of this former industrial site in principle, including a long term conservation outcome for the riparian corridor and retained areas of remnant native vegetation...</p> <p>Retention of Illawarra Lowlands Grassy Woodland Endangered Ecological Community (EEC) in particular, rehabilitation of the existing degraded watercourse and creek realignment works to mitigate flood risk are considered to be environmental priorities.</p>
The National Trust of Australia Illawarra Shoalhaven Regional Branch	<p><i>As heritage will be an important consideration:</i></p> <p><i>a. It is suggested the following aspects be considered when redeveloping the site:</i></p> <ul style="list-style-type: none"> <i>i. Industrial history</i> <i>ii. Archaeology</i> <i>iii. Industrial 'architecture' (see '20th Century Architecture in Wollongong', Robert Irving)</i> <p><i>b. Significant heritage elements be conserved.</i></p>
Sydney Trains	<p>Expressed concern with regard to the proposal. The following concerns were identified:</p> <ol style="list-style-type: none"> Proposed boundary and acquisitions as shown in the submitted documentation by the applicant Traffic assessment and relation to the level crossing, and whether an overbridge would be required Potential land contamination and remediation of the site Hydrology in relation to the rail corridor bridge to the south-east of the site Sydney Trains 33kv Aerial line asset on the western side of the rail corridor and the proximity to proposed development No consultation with Sydney Trains to date by the applicant <p>Sydney Trains has requested that submitted documentation be revised for review, to ensure the structural integrity and continued safe operations of the rail corridor and its assets. The proponent has held discussions with Sydney Trains to progress the matter.</p>

	COMMENT
Transport NSW (TNSW) and RMS	<p>A number of concerns have been raised with respect to the submitted information. Revised documentation and further studies have been requested to address the following concerns:</p> <ul style="list-style-type: none"> • Development in rail corridors; • Intersection: Memorial Drive and Railway Street; • Proposed improvements to Memorial Drive; • Traffic Generation; • Traffic Distribution
Sydney Water	<p>Sydney Water has advised Council that there is limited wet and dry weather capacity issues within the downstream system. They have advised the proponent:</p> <p><i>"If the proponent of the development can provide sufficient on site storage to ensure that pumped flows would only be discharged to Sydney system under dry weather condition, and flows limited to 2L/s or less, then there may be a potential to accommodate proposed development servicing."</i></p>

Community Consultation

Preliminary community consultation was carried out from 23 October – 24 November 2017 and a total of 18 submissions were received with regard to the proposal. The issues and comments raised by the community are noted as follows:

Submission	Comment
Development should incorporate over 55's dwellings within proposed dwelling mix.	Noted. The proponent has noted within the SEE that Seniors Housing may present a highly feasible option and is to be considered further. Furthermore, the R3 Medium density zone would permit Seniors Living.
Overdevelopment of the site.	This comment is noted and has been taken into consideration in the preliminary assessment of the proposal.
24m height limit excessive.	The concern is noted. Additional detailed design and a site specific DCP will be required prior to exhibition.
More parks and open space areas required for the size of the development.	<p>The proponent has advised that the Concept Strategy includes approximately 39% of the site being dedicated to open space, including retention of identified significant native vegetation communities, a riparian corridor and the design of active pathway links throughout the site.</p> <p>However, it is noted that limited open space is identified within the neighbourhood urban concept plan, primarily 'heritage plaza' and a playground within the south of the site. Further design resolution is required through site specific DCP.</p>

Submission	Comment
Concern removal of vegetation/forest within southern portion of the site.	The concern is noted, however, the Concept Plan indicates that the majority of the vegetation within the southern portion of the site will be retained including the identified significant native vegetation community of Moist Blue Gum Blackbutt Forest. Furthermore the proponent is seeking to reactivate the riparian corridor providing active pathway linkages encouraging green links from the coast through the site to Corrimal Town Centre and adjoining community facilities.
Concern's raised as to the inadequacy of the Aboriginal Heritage Report, as it is known that the creek line structures within the Coke Works site were traditional fishing grounds for Aboriginal people in the area. It is recommended that further archaeological consultation with the Aboriginal community be considered.	Comments are noted. Council's assessment considers that the documentation provided in relation to the potential Aboriginal Cultural Heritage Significance of the site is not adequate, and has requested the preparation of an 'Aboriginal Cultural Heritage Assessment' to supplement the submitted documentation.
Increase in traffic and negative impact upon surrounding road network including Railway Street and Railway Crossing.	Comments are noted. Council's Traffic Division has reviewed the preliminary details and requested further investigation be carried out as the proposal is developed. In addition TNSW have also raised a number of concerns and requested additional information and review of supporting documentation.
Concern with regard to current flooding impacts, existing inefficient pipework at the intersection of Railway Street and Memorial Drive, flood affected Railway Street, lack of resolution with regard to current impacts.	Comments are noted. Further detailed information has been requested.
Access to Corrimal town centre via railway street is difficult due to conflicting factors; vehicles, traffic lights, pedestrians, congestion.	Noted, connectivity and linkages between the site and surrounding locality are an important consideration and are being developed further.
Corrimal train station should be considered as an express station for Sydney services. Current timetable forces many residents to drive to Thirroul. The carparks at Thirroul are not sustainable, nor is the increase traffic load between Corrimal and Thirroul.	The time tabling of train service is a matter for Sydney Trains. An express stop would benefit Corrimal residents but also increase demand for parking.
Significant impact, loss of mature trees not in keeping with the Urban Greening Strategy.	The concern is noted, however, it is considered that the proposed works with respect to the riparian corridor and 'green link' corridor are in keeping with the intent of Council's Urban Greening Strategy. Council will be seeking to ensure that the riparian corridor be strengthened and improved ensuring minimal loss of trees and vegetation.
Include an art house cinema.	Noted, idea to be forwarded to proponent for consideration.

Submission	Comment
Object to the rezoning of Lot 126 DP598190 SP2 to R3 Medium residential.	The objection is noted. Council and Sydney Trains have noted that Lot 126 DP598190 - SP2 Infrastructure (Road) will be required to be acquired to ensure adequate land is available to provide the road infrastructure for the future community. As such Lot 126 is not incorporated within the rezoning of the site.
Retention of industrial heritage items and incorporation within sites redevelopment.	Council's Heritage Division and the National Trust have identified that the site comprises significant industrial heritage items and have requested the need to carry out further investigations.
Protection of grey headed flying fox community.	The concern is noted, further studies have been requested to address the potential impacts.

A public meeting was held on Sunday 18 February 2018, and reportedly attended by approximately 50 persons. ICC advised that speakers stated that they are not against the development but want it to be done in such a way that it becomes an asset for Corrimal. Concerns that were raised included traffic and pedestrian road crossing issues, preservation of industrial heritage, damage to local wildlife habitat, flooding issues (including downstream of the site), pressure on local schools and services and a desire that this be a truly sustainable and quality development.

PLANNING AND POLICY IMPACT

Illawarra Shoalhaven Regional Plan (2015)

The *Illawarra Shoalhaven Regional Plan* (the Plan) was released in 2015 by Department of Planning and Environment. The plan sets out to guide strategic planning within the region for the next 20 years.

The plan has set down goals to provide “a region with a variety of housing choices, with homes that meet needs and lifestyles” and also “a region with communities that are strong, healthy and well-connected”, which are relevant to this Planning Proposal.

In particular, this draft Planning Proposal request is supportive of the following directions:

DIRECTION 2.1 Provide sufficient housing supply to suit the changing demands of the region

DIRECTION 2.2 Support housing opportunities close to existing services, jobs and infrastructure in the region's centres

Council is required to plan for a diverse mix of housing that suits the projected growth, changing demographics and market demand particular to their area. This means that in some cases, zonings and planning controls can increase capacity for housing to promote development opportunities. The proposed amendments to Wollongong LEP 2009 seek to facilitate a diverse housing choice within a medium density zoning from single dwelling, semi-detached dwellings, townhouses, attached dwellings and residential flat buildings.

Corrimal has been identified as a potential location for increasing housing opportunity. The Planning Proposal supports the Northern Corridors potential for increase in housing opportunities.

DIRECTION 2.4 Identify and conserve biodiversity values when planning new communities

This direction provides Council with impetus to integrate biodiversity conservation with proposed development outcomes at the strategic planning stage.

Two significant native vegetation communities have been identified and are to be retained within the study area: 0.68 hectares of Moist Blue Gum Blackbutt Forest located along the eastern side of Towradgi Creek to the south of the site; and approximately 0.6 hectares of Coastal Grassy Red Gum Forest in the northwest corner of the site. The existing riparian corridor through the site is significantly

degraded. The proponent seeks to realign the creek to improve the riparian values and significantly reduce flooding of adjoining neighbouring properties.

DIRECTION 3.2 Enhance community access to jobs, goods and services by improving connections between centres and growth areas

DIRECTION 3.3 Build socially inclusive, safe and healthy communities

The subject site has the potential to provide a diverse mix of housing close to existing services offered by Corrimal Town Centre providing connectivity and direct linkage to public transport, public facilities and the coast.

DIRECTION 3.4 Protect the region's cultural heritage

A Heritage Impact Statement has been submitted in support of the draft Planning Proposal request. Heritage investigations have not found evidence of Aboriginal heritage on the site. However, significant post European heritage has been identified and the proponent intends to incorporate the identified structures within the Neighbourhood concept plan within a community pedestrian plaza area adjacent to Corrimal train station.

DIRECTION 5.1 Protect the region's environmental values by focusing development in locations with the capacity to absorb development

The subject site has been identified by Council and the Department as a key site in the northern corridor for increasing housing opportunity adjacent to public transport and in close proximity to Corrimal Town Centre. The proposed redevelopment includes realignment of the riparian corridor protection of identified biodiversity and the potential improvement and maintenance of the green corridor with Corrimal Town Centre and the coast.

Community Strategic Plan – Wollongong 2022

The Wollongong 2022 Community Strategic Plan outlines the community's priorities and aspirations, providing directions for the provision of key projects and services. The relevant objectives and goals are noted as follows:

1 We value and protect our environment

- 1.1 *The natural environment is protected and enhanced.*
- 1.2 *Our coastal areas and waterways are protected and enhanced.*
- 1.3 *Wollongong's ecological footprint is reduced.*
- 1.4 *Community awareness and appreciation of heritage is increased.*
- 1.5 *Local food production and community food initiatives are supported.*
- 1.6 *The sustainability of our urban environment is improved.*

Comment

The draft Planning Proposal request seeks to realign the existing riparian corridor, maintain identified native vegetation communities providing a natural linkage throughout the site whilst encouraging connectivity to public facilities, recreational areas, Corrimal Town Centre and the coast.

The proposal intends to promote the nominated objectives and goals of Council's Community Strategic Plan.

5 We are a healthy community in a liveable space

- 5.1 *There is an increase in the physical fitness, mental health and emotional wellbeing of all our residents.*
- 5.2 *Residents have improved access to a range of affordable housing options.*
- 5.3 *The public domain is maintained to a high standard.*
- 5.4 *Community safety is improved.*
- 5.5 *Participation in recreational and lifestyle activities is increased.*
- 5.6 *Residents have a high level of life satisfaction and personal happiness.*

Comment

The draft Planning Proposal request seeks to provide a diverse housing choice including low scale residential flat buildings, a mix of townhouses, semidetached and semi attached dwellings on smaller lot holdings. The proponent is seeking to deliver housing that is affordable and diverse, highlighting the need for the proposed R3 Medium Density Residential zone. The sites unique location adjacent to Corrimal train station, within proximity to Corrimal Town Centre and various public facilities also incorporates green link corridors, open space areas, walking and cycling paths throughout the site.

The proposal seeks to promote the nominated objectives and goals of Council's Community Strategic Plan.

6 *We have sustainable affordable and accessible transport.*

6.1 *Walking, cycling and public transport is an accessible and well-resourced means of transport, and the use of private cars is reduced.*

6.2 *Wollongong is supported by an integrated transport system.*

6.3 *Transport disadvantaged communities have increased access to services.*

Comment

The site lies adjacent to Corrimal Railway Station and is located 350m from Corrimal Town Centre which will encourage the development of an integrated community. The proposal seeks to develop a pedestrian and cycle network throughout the site whilst also harnessing the opportunity to extend the existing green corridors providing connectivity to Corrimal Town Centre, community and public facilities and the coast.

The proposal promotes the nominated objectives and goals of Council's Community Strategic Plan.

This report contributes to the delivery of Wollongong 2022 goal 5 *'We are a Healthy Community in a Liveable City'*. It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2012-2017	Annual Plan 2017-18
Strategy	5 Year Action	Annual Deliverables
5.15 The long term needs of the community, including our people and our places, are effectively planned for	5.1.5.1 Continue to undertake social, land use and environmental planning activities that assists in service planning	Assess rezoning submissions and progress supported Planning Proposals

Corrimal Town Centre Plan 2015-2025

The Corrimal Town Centre Plan 2015-2025 provides strategic guidance on the future of Corrimal Town Centre. The Vision of this Plan seeks to create a future Corrimal Town Centre that has:

- A Distinct Identity
- A Thriving Community Heart (Memorial Park)
- Strong Connections
- Smart Growth
- Pride in Quality Spaces

The report notes that Corrimal is strategically placed to grow. As a major urban centre close to the Wollongong City Centre, Corrimal is accessible by road and public transport. Corrimal provides a wide variety of retail, services and community facilities for residents of the area and the northern suburbs. Population growth in Corrimal and surrounds will bring more people to the centre and increase demand for retail variety, services and housing. Growth needs to be of high-quality design that adds to the overall ambience of the locality whilst enhancing resident and visitor experiences.

The Plan specifically identified the subject site as a 'key development site' noting:

"4. Former Coke Works Site

- *Large site, adjacent to railway station, walking distance to centre services, although Memorial Drive crossing is not pleasant*
- *Investigations are required to understand flooding and contamination constraints. This will inform development capacity of the site.*
- *Important to establish a quality east-west pedestrian link from Town Centre to railway station and beach. Development would need to have an active street interface with Railway Street.*
- *Large area currently zoned RE2 Private Recreation. Remaining portion zoned IN3 Heavy Industrial. Future development would need to be complementary to Town Centre and not draw away activity from Town Centre*
- *Investigations into historical, archaeological and cultural significance of site required*
- *Future works need to be cognisant of flood impacts and in accordance with existing Vegetation Management Plan. Opportunities for open space and recreation uses in flood-prone areas."*

It is considered that the Planning Proposal is generally supportive with the intent and direction of the revitalisation strategy.

Urban Greening Strategy 2017-2037

The *Urban Greening Strategy 2017-2037* aims to strategically increase the quality and quantity of all vegetation in an urban setting. The Strategy identifies opportunities for our City to harness the benefits of trees and vegetation, contributing towards creating a world class urban landscape.

The **Vision** is that:

Wollongong will grow and nurture a healthy, diverse and well-managed urban forest to deliver a renewed and resilient place for people, enterprise and the ecosystems that support us.

The following Principles will inspire and inform Council's approach to urban greening in Wollongong:

- more strategic and targeted urban greening;
- evidence-based decision making and programming;
- enhanced amenity of public spaces;
- maximisation of ecosystem services and biodiversity connections;
- stronger leadership and partnerships with the community; and
- greater diversity of urban greening.

The draft Planning Proposal request is conducive to the Strategy as it seeks to enhance and maintain existing identified biodiversity on the site, providing connectivity and linkage to an existing 'green link' which will ultimately connect Corrimal Town Centre with the coast. Furthermore, the proposal will contribute to increasing the canopy cover of the City.

Illawarra Biodiversity Strategy 2011

A key requirement of the Illawarra Biodiversity Strategy is to *'improve the integration of biodiversity conservation into planning instruments'*.

The draft Planning Proposal request seeks to retain approximately 39% of the site as public open space (to be rezoned RE1 Public Recreation) identifying areas of isolated significant native vegetation communities within the site and seeking to provide a balanced retention of a degraded vegetation corridor by encouraging connectivity and formalising an open space green corridor.

The proponent has submitted a supporting Flora and Fauna Assessment which highlights inconsistencies between the existing mapped Biodiversity and has identified significant value biodiversity of the subject site. As such the proponent is seeking to remove the land as identified within the Wollongong LEP 2009 'Natural Resource Sensitivity – Biodiversity Map'. This is noted, however it is considered that since the investigations have identified two significant native vegetation communities being 'Moist Blue Gum Black Forest' approximately 0.68 hectares in the southern section of the site and 'Coastal Grassy Red Gum Forest' approximately 0.61 hectares in the north west corner of the site, these

areas should be accurately mapped and the Natural Resource Sensitivity – Biodiversity Map amended accordingly.

Council's Environment Officer reviewed the proposal noting general support for the retention of the majority of the significant native vegetation and the amendment to the Biodiversity Map.

Development near Rail Corridors and Busy roads – Interim Guideline

The Department's Guideline seeks to assist in the planning, design and assessment of development in, or adjacent to, rail corridors and busy roads. It also supports specific rail and road provisions of the State Environmental Planning Policy (Infrastructure) 2007 (the 'Infrastructure SEPP').

The key objectives of these provisions are to:

- *protect the safety and integrity of key transport infrastructure from adjacent development; and*
- *ensure that adjacent development achieves an appropriate acoustic amenity by meeting the internal noise criteria specified in the Infrastructure SEPP.*

Resident's quality of life can be adversely affected unless appropriate site layout, design or other mitigation measures to minimise noise, vibration and air quality impacts have been integrated into the proposed development. A minimum 20 metre setback is the principal measure proposed to the residential development adjacent to the rail corridor to account for noise and vibration, whilst the realignment of the riparian corridor and RE1 Public Recreation zoning is proposed as a buffer to Memorial Drive.

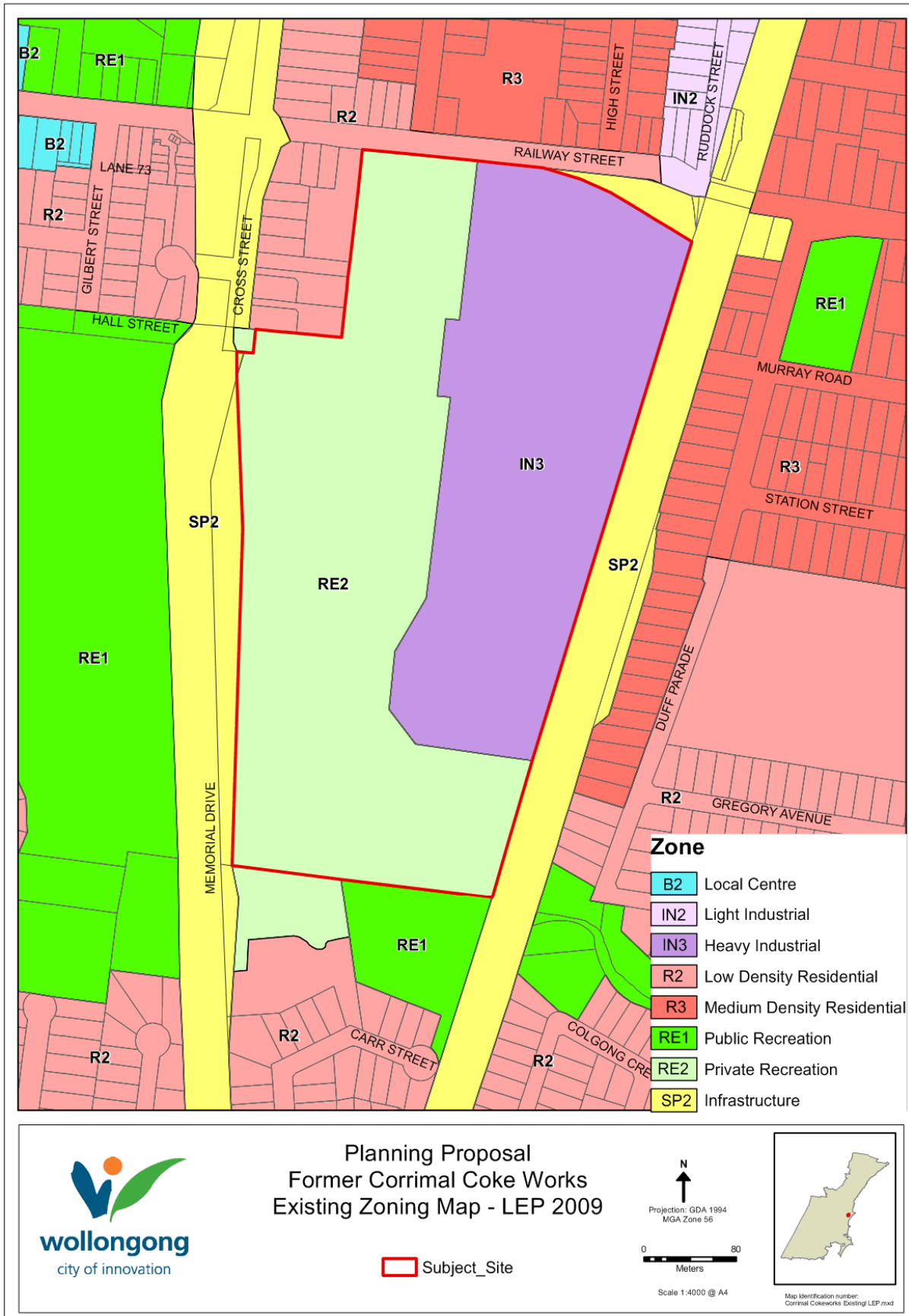
The Guidelines also seek to better integrate land use and transport, with increasing residential densities and business activities near rail corridors. This Guideline is of relevance to development near railway stations as it seeks to promote the location of affordable housing and concentrate business activities near stations which in-turn improves accessibility and opportunities for increased rail patronage. This is particularly highlighted where housing or business is within easy walking distance from the station.

As its primary impetus the draft Planning Proposal request is seeking to highlight the connectivity of the site with Corrimal Train Station and Corrimal Town Centre whilst creating a residential hub adjacent to Corrimal Train Station. Additional detail design will be required to be submitted to ensure that acoustic and vibration standards are complied with throughout the development.

CONCLUSION

The draft Planning Proposal comprises strategic merit to progress to Gateway subject to the provision of additional detail prior to public exhibition. It is therefore recommended that Council support the progression of the draft Planning Proposal and submit it to the NSW Department of Planning and Environment seeking a Gateway determination enabling public exhibition.







PROJECT:
CORRIMAL COKE WORKS

CLIENT:
ILLAWARRA COKE COMPANY

DRAWING:
INDICATIVE CONCEPT PLAN

DATE:
9 FEBRUARY 2018

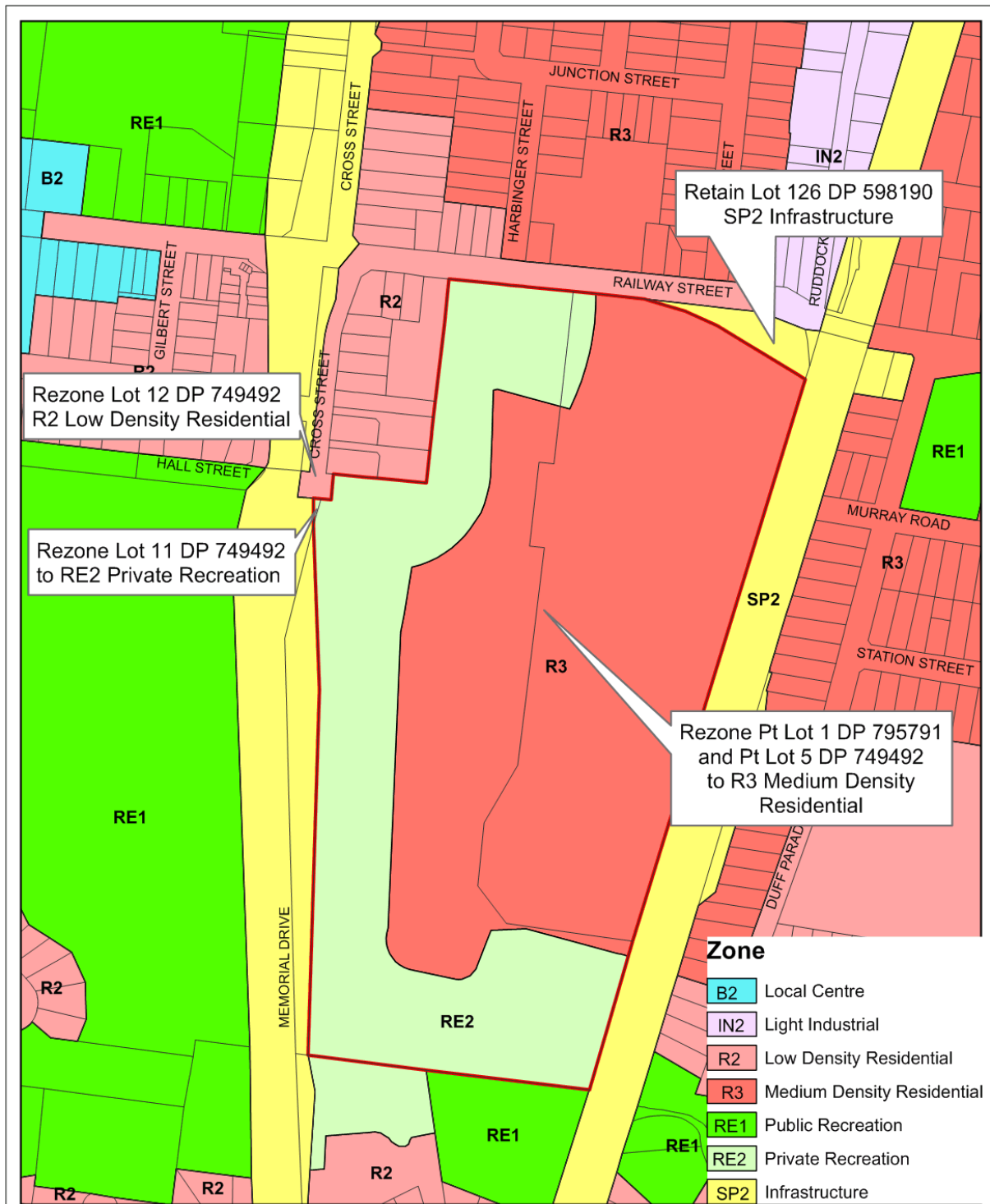
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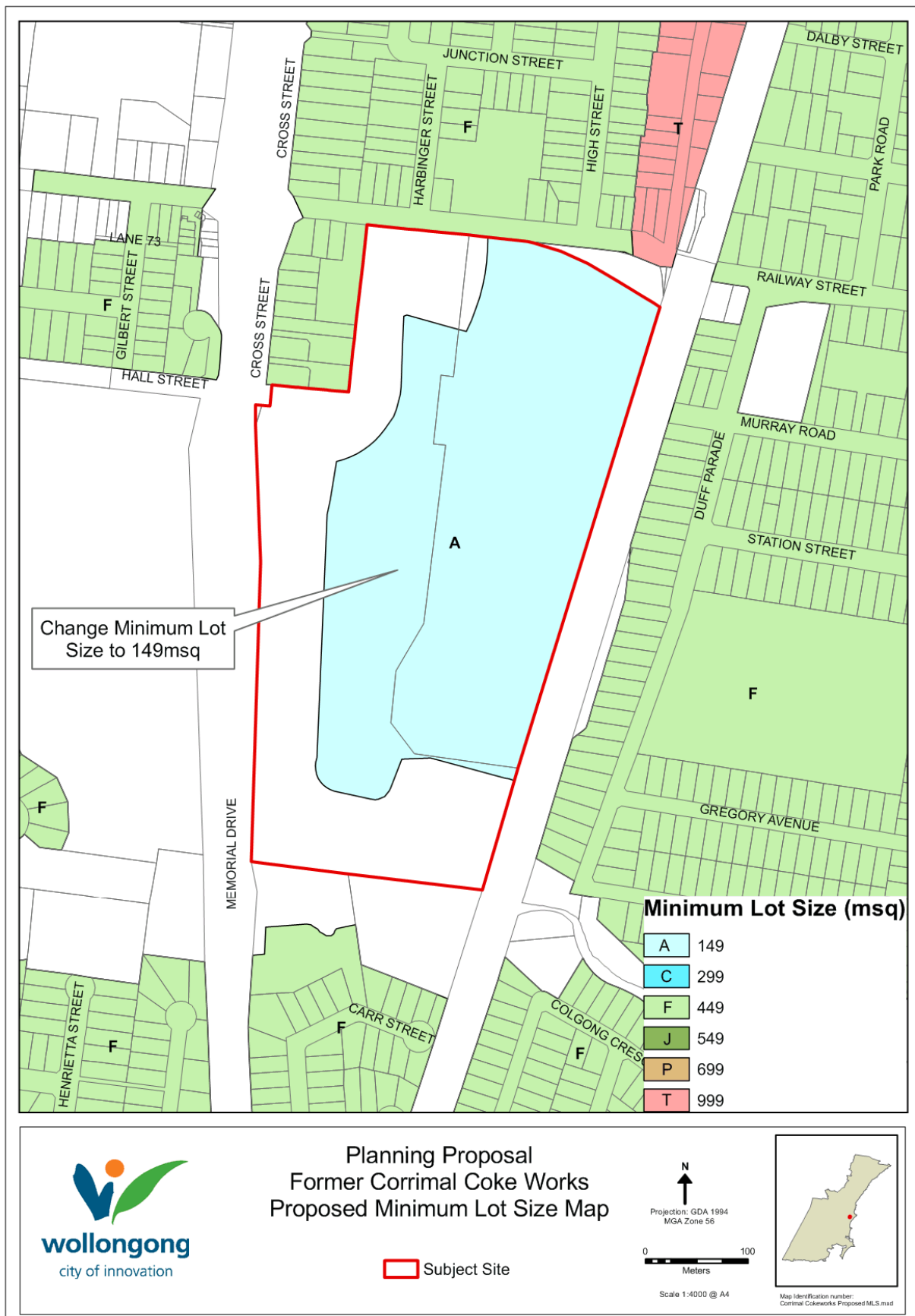
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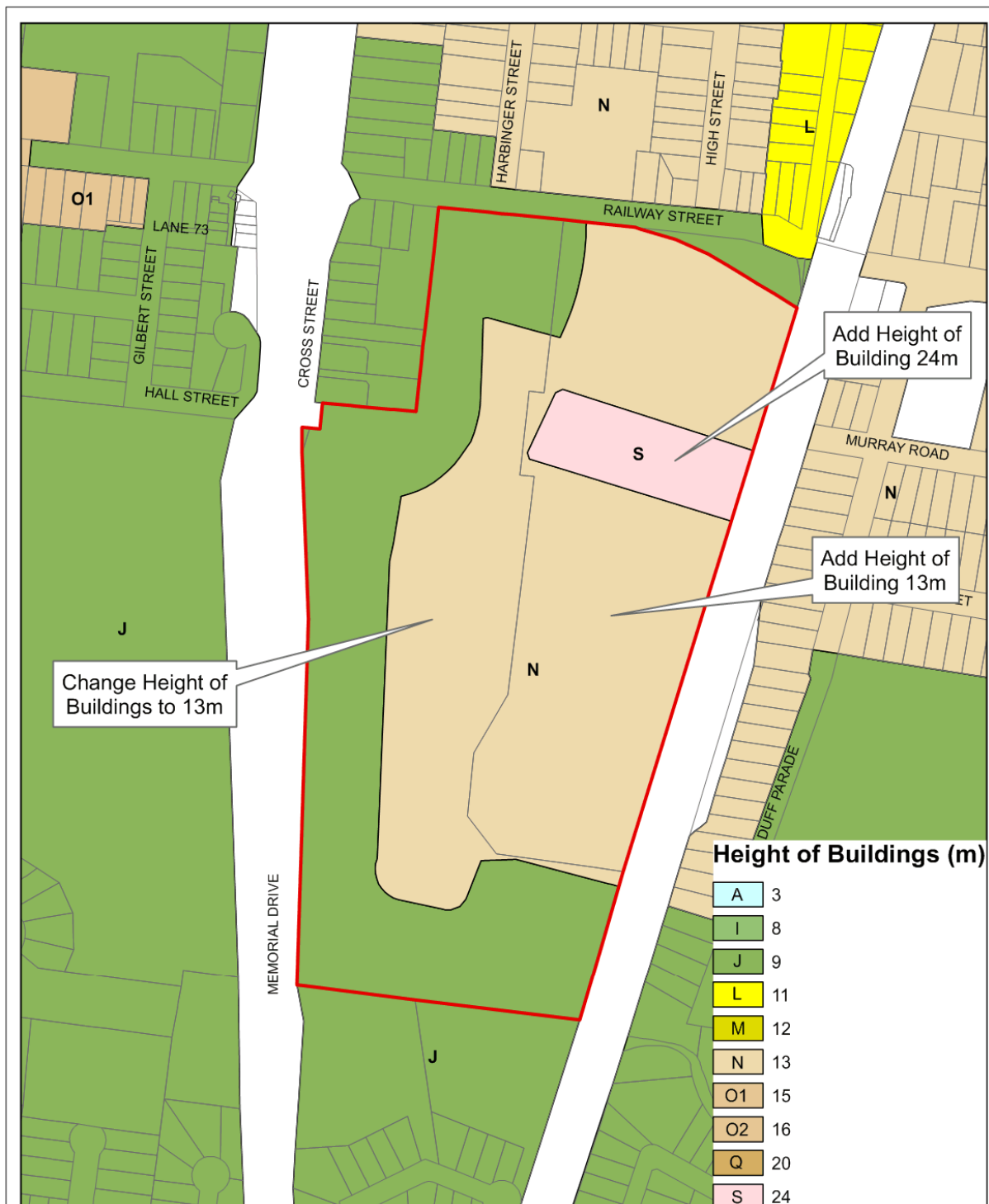
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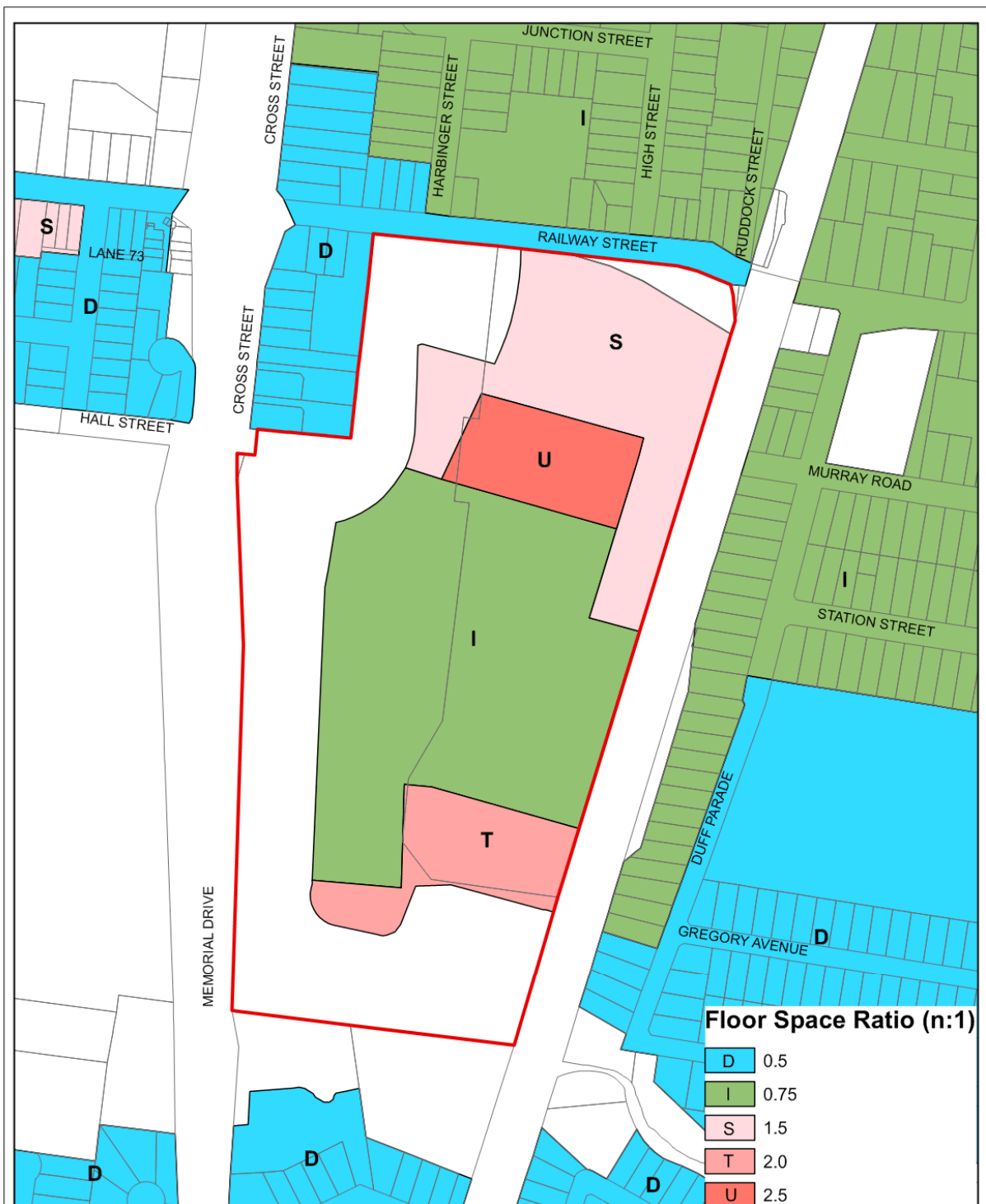
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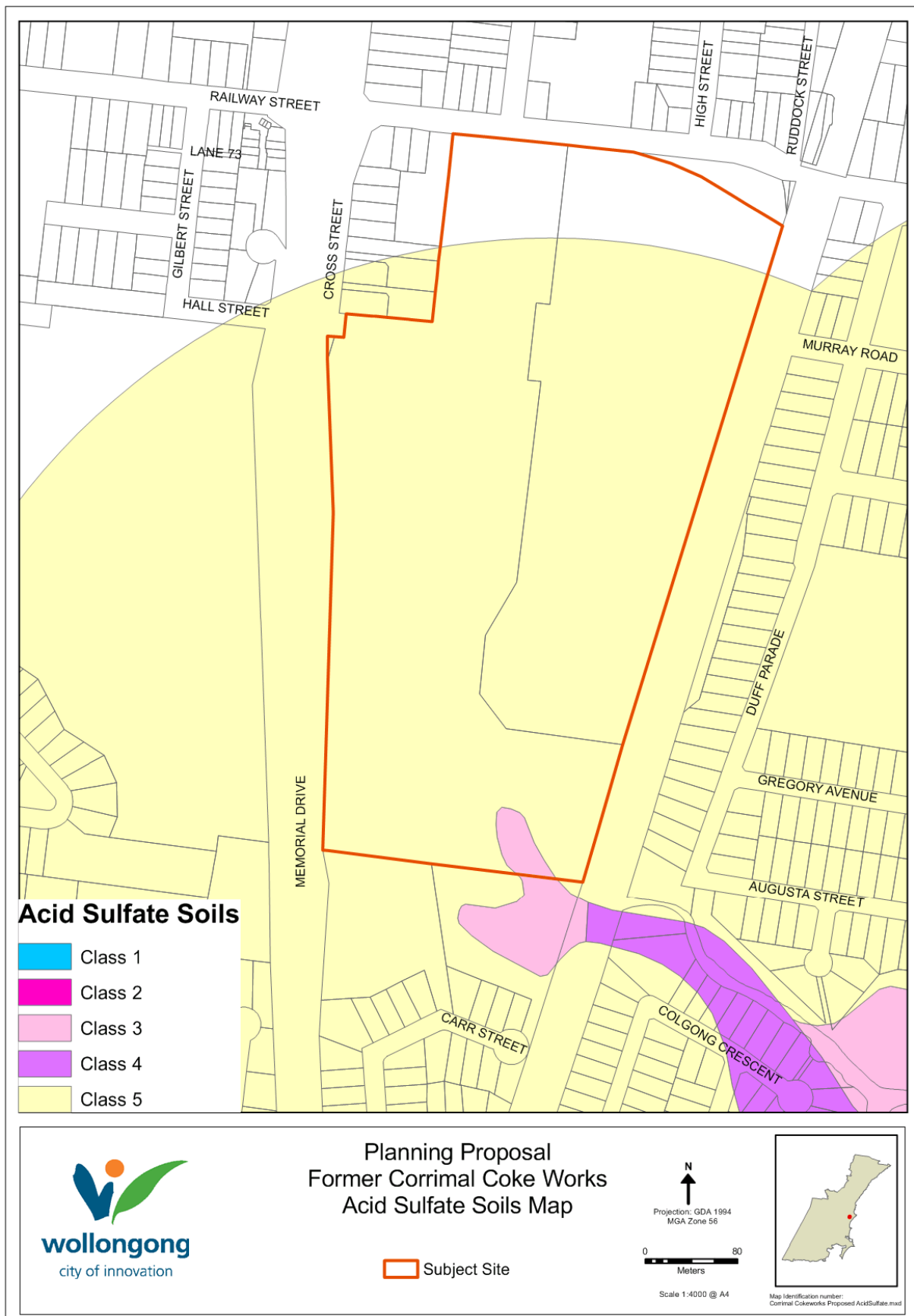
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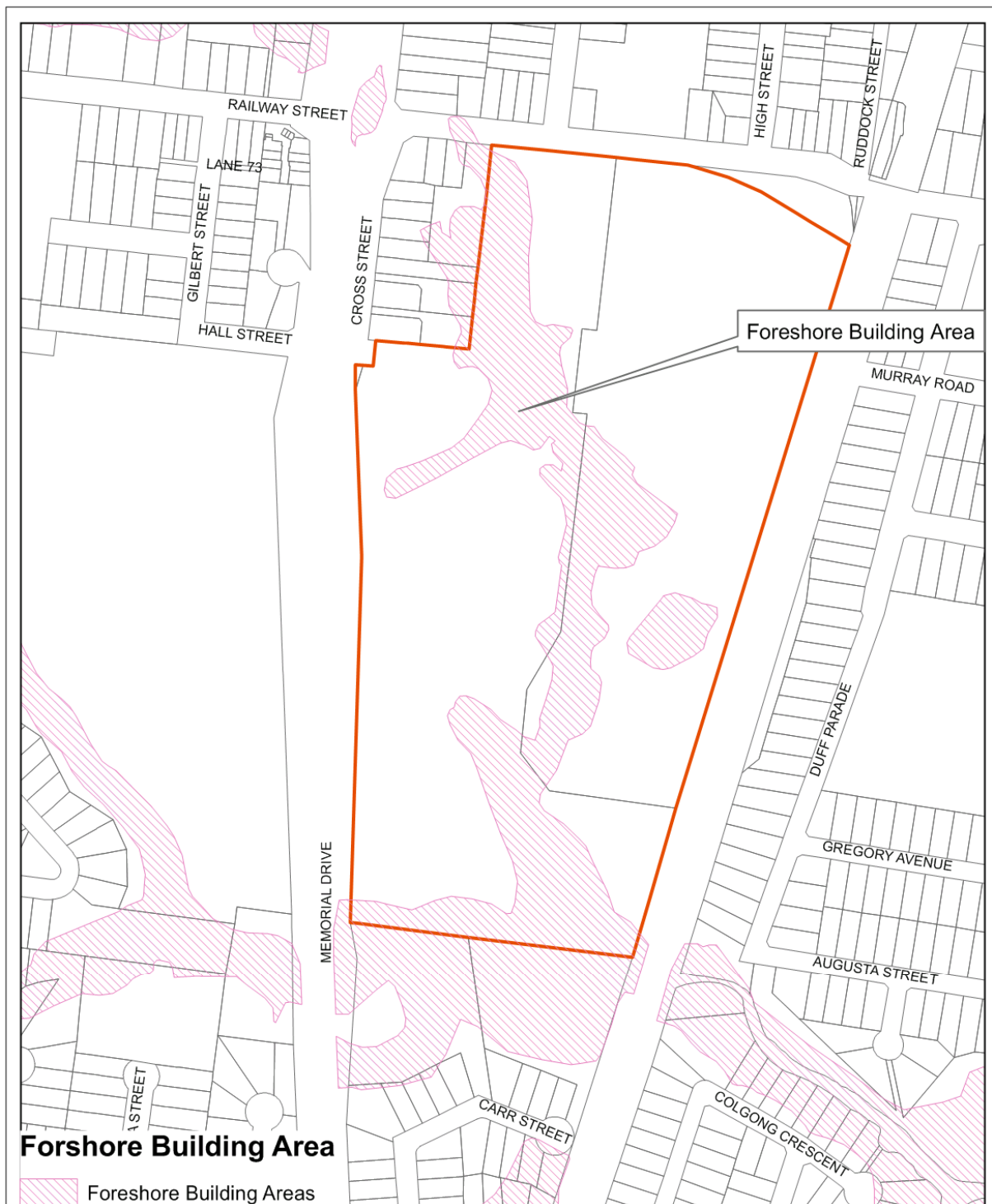












Foreshore Building Area

Foreshore Building Areas

